

SCOTTSDALE AIRPORT

REHABILITATE GATE 1 TAXILANE AND BYPASS TAXILANE AND IMPLEMENT DRAINAGE IMPROVEMENTS

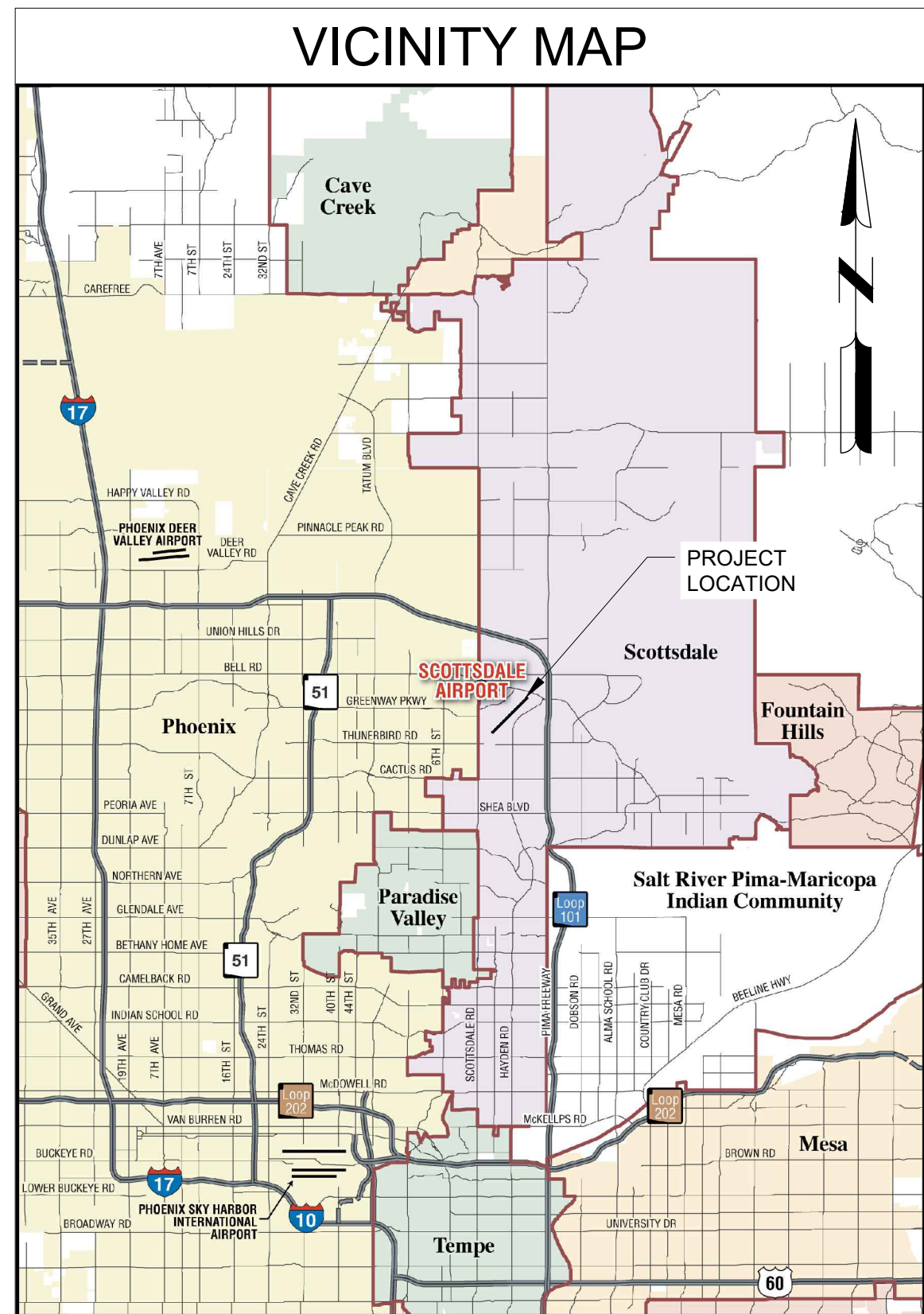
15000 N AIRPORT DRIVE

SCOTTSDALE, ARIZONA, 85260

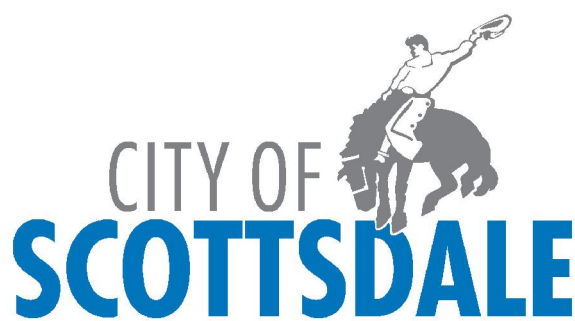
FAA AIP NO.: 3-04-0032-049-2025

ISSUE FOR BID

MARCH 2025



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PROJECT DESCRIPTION

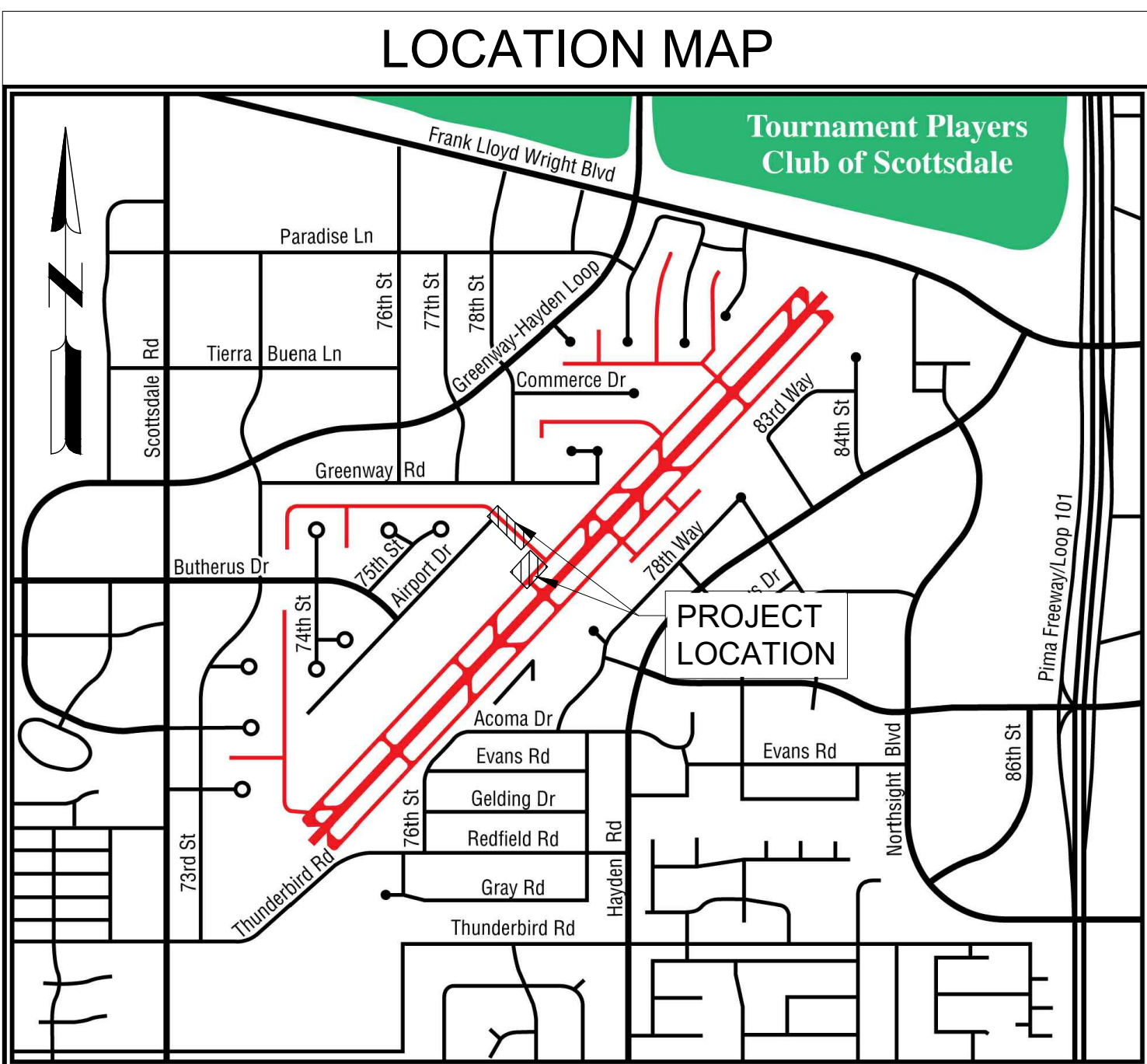
ADOT NO.: E6S1A01C
CITY PROJECT NO.:XXXX
CITY BID NO.: IFB-032025-249

GATE 1 TAXILANE REHABILITATION

- PLACE TRAFFIC CONES AND BARRICADES
- TAXILANE PAVEMENT REMOVAL, 3-INCH DEPTH
- REMOVE AND REPLACE AIRCRAFT ACCESS GATE LOOP DETECTOR
- PAVE ASPHALT SURFACE COURSE, 3-INCH DEPTH
- APPLY PAVEMENT MARKINGS

BYPASS TAXILANE PAVEMENT REHABILITATION

- PLACE TRAFFIC CONES AND BARRICADES
- PERIMETER ROAD PAVEMENT REMOVAL, FULL DEPTH
- TAXILANE PAVEMENT REMOVAL, 2.5-INCH DEPTH
- PAVE ASPHALT SURFACE COURSE, 2.5-INCH DEPTH
- SCARIFY AND GRADE FULL DEPTH PAVEMENT REMOVAL AREA.
- PLACE AND COMPACT NEW CASPR WITHIN EXISTING PERIMETER ROAD
- APPLY PAVEMENT MARKINGS



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IMPLEMENT DRAINAGE IMPROVEMENTS
15000 N AIRPORT DRIVE
SCOTTSDALE, ARIZONA, 85260

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AIP NO.: 3-04-0032-049-2025
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DATE: MARCH 2025
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SHEET CONTENTS
COVER SHEET

SHEET NO. 1 of 14

G-001

LEGEND

SYMBOLS

	CONTROL POINT
	CLEANOUT, SANITARY OR STORM
	ELECTRIC HANDHOLE (PULLBOX)
	SANITARY SEWER MANHOLE
	SOIL BORING LOCATION
	CATCH BASIN, SQUARE
	STORM DRAIN MANHOLE
	UTILITY CROSSING / CONFLICT or CAUTION
	WATER MANHOLE
	WATER METER
	WATER VALVE
	SECTION REFERENCE
x 738.76'	EXISTING SPOT ELEVATION
x [738.76 FG]	FINISHED / PROPOSED SPOT ELEVATION
	FINISHED / PROPOSED SLOPE
	RUNWAY EDGE LIGHT (LENS COLOR)
	TAXIWAY EDGE LIGHT (LENS COLOR)
	RUNWAY END IDENTIFIER LIGHT
	PRECISION APPROACH PATH INDICATOR
	ELECTRICAL PULL BOX
	JUNCTION CAN
	INLET PROTECTION
	PORTABLE TRAFFIC DELINEATOR
	TRAFFIC CONE
	GATE GUARD
	WINDCONE
	TIEDOWN
	BOLLARD
	FIRE HYDRANT

LINES

	PROJECT LIMIT
	GAS
	EXISTING CONTOUR LINES
	PROPOSED CONTOUR LINES
	FENCE, CHAIN-LINK
	GRADE BREAK
	HAUL ROUTE
	PROPERTY LINE
	SANITARY SEWER
	STORM DRAIN / CULVERT
	COMMUNICATIONS
	UNDERDRAIN
	WATER
	RUNWAY OBJECT FREE AREA
	RUNWAY SAFETY AREA
	TAXIWAY OBJECT FREE AREA
	TAXIWAY SAFETY AREA
	LOW-PROFILE BARRICADES
	FIBER OPTIC CABLE, UNDERGROUND
	EXISTING ELECTRIC, UNDERGROUND
	BLACK OUT (E) PAVEMENT MARKING
	SAWCUT

PATTERNS

	PROJECT WORK AREA
	(E) AGGREGATE BASE COURSE (SECTION VIEW)
	(E) SUBGRADE (SECTION VIEW)
	(E) CEMENT TREATED SOIL BASE COURSE (SECTION VIEW)
	(E) ASPHALT SURFACE COURSE (SECTION VIEW)
	(N) ASPHALT SURFACE COURSE (SECTION VIEW)
	(N) ASPHALT SURFACE COURSE (3-INCH DEPTH) (PLAN VIEW)
	(N) ASPHALT SURFACE COURSE (2.5-INCH DEPTH) (PLAN VIEW)
	(E) CASPR
	PLACE AND COMPACT (N) CASPR
	PAVEMENT REMOVAL, 2.5-INCH DEPTH
	PAVEMENT REMOVAL, 3-INCH DEPTH
	PAVEMENT REMOVAL, FULL DEPTH

GENERAL ABBREVIATIONS

AAC	ARIZONA ADMINISTRATIVE CODE
AB	AGGREGATE BASE
AC	ASPHALT CONCRETE
ACP	ASBESTOS CEMENT PIPE
ADA	AMERICAN DISABILITIES ACT
ADOT	ARIZONA DEPARTMENT OF TRANSPORTATION
ALT	ALTERNATE
AOA	AIR OPERATIONS AREA
APCH	APPROACH
APPROX	APPROXIMATE
APS	ARIZONA PUBLIC SERVICE
ARFF	AIRPORT RESCUE AND FIRE FIGHTING
ASB	AGGREGATE SUBBASE
ASOS	AUTOMATED SURFACE OBSERVING SYSTEM
AR	ACCESS ROAD
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS
ATCT	AIR TRAFFIC CONTROL TOWER
ATO	AIR TRAFFIC ORGANIZATION
AWG	AMERICAN WIRE GAUGE
AWWA	AMERICAN WATER WORKS ASSOCIATION
AZ	ARIZONA
BC	BEGINNING OF CURVE
BIT	BITUMINOUS
BLDG	BUILDING
BM	BENCHMARK
BOT	BOTTOM
BVC	BEGINNING OF VERTICAL CURVE
C-C	CENTER TO CENTER
C	CIVIL
CABC	CRUSHED AGGREGATE BASE COURSE
CASPR	CRUSHED AGGREGATE SLOPE PROTECTION ROCK
CB	CATCH BASIN
CIPCP	CAST-IN-PLACE CONCRETE PIPE
CJ	CONSTRUCTION JOINT
CFS	CUBIC FEET PER SECOND
CL	CENTERLINE
CLF	CHAIN-LINK FENCE
CLR	CLEAR
CMP	CORRUGATED METAL PIPE
CO	CLEANOUT
CONC	CONCRETE
CONT	CONTINUOUS
COS	CITY OF SCOTTSDALE
CP	CONTROL POINT
CTB	CEMENT-TREATED BASE
CY	CUBIC YARDS
DB	DIRECT BURIAL
DEG	DEGREE
DG	DECOMPOSED GRANITE
DI	DROP INLET
DIA	DIAMETER
DIM	DIMENSION
DIP	DUCTILE IRON PIPE
DP	DEPTH
DR	DRIVE
DSPM	DESIGN STANDARDS AND POLICIES MANUAL
DTL	DETAIL
(E)	EXISTING
E	ELECTRICAL LINE
EA	EACH
EC	END OF CURVE
EG	EXISTING GRADE
EL, ELEV	ELEVATION
EOP	EDGE OF PAVEMENT
EVC	END OF VERTICAL CURVE
FAA	FEDERAL AVIATION ADMINISTRATION
FBO	FIXED BASE OPERATOR
FDC	FIRE DEPARTMENT CONNECTION
FEMA	FEDERAL EMERGENCY MANAGEMENT AGENCY
FF	FINISHED FLOOR
FG	FINISHED GRADE
FH	FIRE HYDRANT
FL	FLOW LINE
FM	SANITARY SEWER FORCE MAIN
FOC	FIBER OPTIC CABLE

FPS	FEET PER SECOND
FT	FEET
G&D	GRADING AND DRAINAGE
G	NATURAL GAS LINE
GAL	GALLON
GALV	GALVANIZED
GB	GRADE BREAK
GND	GROUND
GPM	GALLONS PER MINUTE
GS	GROUND SHOT
H	HEIGHT
HDPE	HIGH DENSITY POLYETHYLENE
HIRL	HIGH INTENSITY RUNWAY LIGHT
HITHL	HIGH INTENSITY THRESHOLD LIGHT
HORIZ	HORIZONTAL
HMA	HOT MIX ASPHALT
HP	HIGH POINT
HW	HEADWALL
IE	INVERT ELEVATION
IN	INCHES
INC	INCORPORATED
INV	INVERT
IP	IN-PAVEMENT
L	LENGTH
LBS	POUNDS
LF	LINEAL FEET
LOC	LOCALIZER
LWL	LOW WATER LEVEL
MAG	MARICOPA ASSOCIATION OF GOVERNMENTS
MCESD	MARICOPA COUNTY ENVIRONMENTAL SERVICES DEPARTMENT
MG	MILLED GRADE
MGR	MANAGER
MH	MANHOLE
MAX	MAXIMUM
MID	MIDPOINT
MIN	MINIMUM
MALS	MEDIUM APPROACH LIGHTING SYSTEM
MIRL	MEDIUM INTENSITY RUNWAY LIGHT
MITL	MEDIUM INTENSITY TAXIWAY LIGHT
MPH	MILES PER HOUR
(N)	NEW
N	NORTH, NORTHING
NE	NORTHEAST
NIC	NOT IN CONTRACT
NO.	NUMBER
NOTAM	NOTICE TO AIR MISSIONS
NSF	NATIONAL SANITATION FOUNDATION
NTS	NOT TO SCALE
NW	NORTHWEST
OC	ON CENTER
OFA	OBJECT FREE AREA
OFF	OFFSET
OH	OVERHEAD
OWS	OIL WATER SEPARATOR
PAPI	PRECISION APPROACH PATH INDICATOR
PB	PULL BOX
PC	POINT OF CURVATURE
PCC	PORTLAND CEMENT CONCRETE
PCF	POUNDS PER CUBIC FOOT
PERF	PERFORATED
PI	POINT OF INTERSECTION
POB	POINT OF BEGINNING
POC	POINT OF CURVE
POE	POINT OF ENDING
PRV	PRESSURE RELIEF VALVE
PSI	POUNDS PER SQUARE INCH
PSF	POUNDS PER SQUARE FOOT
PT	POINT OF TANGENCY
PVC	POLYVINYL CHLORIDE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENCY
PWR	PERMISSION TO WORK IN RIGHT OF WAY
Q	RATE OF FLOW
QTY	QUANTITY

R&R	REMOVE AND REPLACE
R	RADIUS
RAIL	RUNWAY ALIGNMENT INDICATOR LIGHT
RC	RELATIVE COMPACTION
RCP	REINFORCED CONCRETE PIPE
RD	ROAD
REIL	RUNWAY END IDENTIFIER LIGHT
REL	RELOCATE EXISTING
REQ	REQUIRED
RGL	RUNWAY GUARD LIGHT
RET	RETAINING
ROFA	RUNWAY OBJECT FREE AREA
ROW	RIGHT OF WAY
RSA	RUNWAY SAFETY AREA
RWA	RESTRICTED WORK AREA
RWAPP	RUNWAY APPROACH LIGHT
RWY	RUNWAY
S&W	SEWER AND WATER
SDR	STANDARD DIMENSION RATIO
SF	SQUARE FOOT
SG	STRAIGHT GRADE
SH	SHOULDER
SD	STORM DRAIN
SPEC	SPECIFICATION
SS	SANITARY SEWER
ST	STREET
STA	STATION
STD	STANDARD
STE	SUITE
STL	STEEL
SW	SOUTHWEST
SY	SQUARE YARD
T	TELEPHONE LINE
TC	TOP OF CURB
TG	TOP OF GRATE
TXL	TAXILANE
TLOFA	TAXILANE OBJECT FREE AREA
TOE	TOE OF BANK
TOP	TOP OF BANK
TDZ	TOUCHDOWN ZONE
TOFA	TAXIWAY OBJECT FREE AREA
TRANS	TRANSFORMER
TSA	TAXIWAY SAFETY AREA
TWY	TAXIWAY
TYP	TYPICAL
UD	UNDERDRAIN
UG	UNDERGROUND
UON	UNLESS OTHERWISE NOTED
UV	ULTRA VIOLET
V	VELOCITY
VC	VERTICAL CURVE
VCP	VITRIFIED CLAY PIPE
VERT	VERTICAL
VG	VALLEY GUTTER
VIF	VERIFY IN FIELD
WL	WATER LINE
W	WEST
W/	WITH
W/O	WITHOUT
WSE	WATER SURFACE ELEVATION
WSP	WELDED STEEL PIPE
WV	WATER VALVE
WWM	WELDED WIRE MESH

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SHEET CONTENTS
LEGEND AND
ABBREVIATIONS

SHEET NO. 2 of 14

G-002

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GENERAL NOTES:

1. ALL WORK REQUIRED TO COMPLETE THE CONSTRUCTION COVERED BY THIS PLAN SHALL BE IN ACCORDANCE WITH FAA STANDARD SPECIFICATIONS UNLESS SPECIFIED OTHERWISE.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MEANS, METHODS, SEQUENCING AND SAFETY USED DURING CONSTRUCTION UNLESS SPECIFICALLY ADDRESSED OTHERWISE.
3. THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS AND REGULATIONS APPLICABLE TO THE CONSTRUCTION COVERED BY THIS PLAN.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING AND COMPLYING WITH ALL PERMITS REQUIRED TO COMPLETE THE WORK COVERED BY THIS PLAN.
5. THE QUANTITIES AND SITE CONDITIONS DEPICTED IN THIS PLAN ARE FOR INFORMATIONAL PURPOSES ONLY. CONTRACTOR SHALL SATISFY THEMSELVES AS TO ACTUAL QUANTITIES AND SITE CONDITIONS PRIOR TO BIDDING THE WORK COVERED BY THIS PLAN.
6. A REASONABLE EFFORT HAS BEEN MADE TO SHOW THE LOCATION OF EXISTING UNDERGROUND FACILITIES IN THE CONSTRUCTION AREA. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGE TO THE EXISTING FACILITIES CAUSED DURING THEIR CONSTRUCTION OPERATIONS. CONTRACTOR SHALL CONTACT ARIZONA 811 AND NOTIFY AIRPORT OPERATIONS PRIOR TO ANY EXCAVATION.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COORDINATION OF THE CONSTRUCTION AFFECTING UTILITIES AND THE COORDINATION OF ANY NECESSARY UTILITY RELOCATION OR MODIFICATIONS.
8. THE CONTRACTOR SHALL VERIFY BY SURVEY THE LOCATION, ELEVATION AND CONDITION OF ALL EXISTING SURFACES AT TIE-IN POINTS AND MATCHLINES PRIOR TO COMMENCEMENT OF CONSTRUCTION. SHOULD DIFFERENCES GREATER THAN 1/2" INCH BE FOUND FROM THAT DEPICTED ON THIS PLAN, RESULTING IN THE DESIGN INTENT REFLECTED ON THIS PLAN NOT ABLE TO BE CONSTRUCTED, CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER FOR DIRECTION ON HOW TO PROCEED PRIOR TO COMMENCEMENT AND/OR CONTINUATION OF CONSTRUCTION.
9. DISPOSAL OF EXCAVATED OR WASTE MATERIAL SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. CONTRACTOR SHALL ASSUME ALL EXCESS MATERIAL WILL BE REQUIRED TO BE HAULED AND DISPOSED OFF AIRPORT PROPERTY. ALL COSTS ASSOCIATED WITH OFF HAUL (INCLUDING ANY ENVIRONMENTAL TESTING) SHALL BE THE CONTRACTOR'S RESPONSIBILITY.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL BARRICADING AND TRAFFIC CONTROL REQUIRED TO MAINTAIN SAFE AND SECURE ACCESS AT THE PROJECT SITE. CONTRACTOR'S ACCESS AND TRAFFIC CONTROL PLAN SHALL BE IN ACCORDANCE WITH THE CONSTRUCTION SAFETY AND PHASING PLAN.
11. THE CONTRACTOR SHALL PROTECT ALL AIRFIELD SIGNAGE, LIGHTING, FENCING, AND SECURITY EQUIPMENT WITHIN THE PROJECT LIMITS. DAMAGE TO EXISTING ELECTRICAL EQUIPMENT, SIGNAGE, FENCING, SECURITY EQUIPMENT OR ANY UNDERGROUND UTILITY CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
12. THE CONTRACTOR SHALL MAKE REQUESTS FOR FACILITY CLOSURES A MINIMUM OF 48 HOURS IN ADVANCE OF SUCH CLOSURES. CONTRACTOR SHALL MAKE REQUESTS FOR RUNWAY CLOSURES TO THE AIRPORT A MINIMUM OF 7 CALENDAR DAYS IN ADVANCE OF SUCH CLOSURES. THE AIRPORT WILL HAVE THE FINAL APPROVAL AUTHORITY OF SUCH CLOSURES. THE AIRPORT RETAINS THE RIGHT TO CANCEL ANY SUCH CLOSURES IN THE INTEREST OF AIRPORT SAFETY AND/OR CRITICAL OPERATIONS.
13. ACCESS TO ALL FIRE HYDRANTS AND PIPESTANDS SHALL BE MAINTAINED AT ALL TIMES.
14. DUMP TRUCKS SHALL USE LOAD COVERS AND SHALL BE LOADED BY THE CONTRACTOR SUCH THAT NO SPILLAGE OCCURS DURING TRANSIT ON STATE, MUNICIPAL, OR AIRPORT ROADWAYS. PRIOR TO BEING ALLOWED ON ANY AIRFIELD PAVEMENT OR ROADWAYS, WHEELS OF TRUCKS SHALL BE CLEANED AND INSPECTED, CONTRACTOR SHALL BE RESPONSIBLE FOR THE COST OF THE IMMEDIATE CLEANING OF MATERIAL TRACKING AND SPILLS ON THE PAVED SURFACES RESULTING FROM THE CONTRACTORS OPERATIONS.
15. THE LOCATION OF HAUL ROUTES ON THE AIRPORT SHALL BE AS SHOWN ON THE PLANS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES WITH THE PARTY HAVING JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE HAUL ROUTES WILL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED TO THEIR ORIGINAL CONDITION UPON COMPLETION OF WORK, OR AS DEEMED NECESSARY BY THE AIRPORT OR ENGINEER. FENCING, DRAINAGE, GRADING, OR OTHER WORK NECESSARY TO CONSTRUCT HAUL ROUTES ON THE AIRPORT IS THE CONTRACTOR'S RESPONSIBILITY AND MUST BE APPROVED BY THE ENGINEER PRIOR TO THE WORK.
16. CONCRETE, RUBBLE, ASPHALT MILLINGS, AGGREGATE BASE, SOILS AND OTHER EXCESS EXCAVATION NOT USED IN THE WORK SHALL BE DISPOSED OFF AIRPORT PROPERTY ACCORDING TO THE LOCAL LAWS AND REGULATIONS. NO MATERIAL SHALL BE WASTED OR STOCKPILED ON THE AIRPORT UNLESS APPROVED BY THE AIRPORT OR ENGINEER.
17. THE CONTRACTOR SHALL NOT ENTER ONTO ANY AREA OUTSIDE OF THE CONSTRUCTION LIMITS, STAGING AREA, OR DESIGNATED HAUL ROUTES WITHOUT THE WRITTEN APPROVAL OF THE AIRPORT.
18. ALL CONSTRUCTION VEHICLE ACTIVITY IN THE AIRCRAFT OPERATION AREA (AOA) SHALL BE CONTROLLED BY THE CONTRACTOR IN ACCORDANCE WITH THE APPROVED CONSTRUCTION SAFETY & PHASING PLAN. SPECIAL RESTRICTIONS APPLY IN AREAS WITHIN THE RUNWAY AND TAXIWAY SAFETY AREAS AND OBJECT FREE AREAS. NO WORK SHALL PROCEED WITHOUT THE APPROVAL OF THE AIRPORT. ALL WORK CREWS SHALL BE ACCOMPANIED BY AN AIRPORT OPERATIONS STAFF EQUIPPED WITH A RADIO CAPABLE OF MONITORING APPROPRIATE AIRPORT FREQUENCIES IF LOCATED INSIDE ACTIVE RUNWAY AND TAXIWAY SAFETY AREAS.

19. CONSTRUCTION EQUIPMENT AND PERSONNEL SHALL YIELD TO AIRCRAFT TRAFFIC AT ALL TIMES.
20. ACTIVITIES PERFORMED BY THE CONTRACTOR REQUIRING THE USE OF CONSTRUCTION EQUIPMENT WITH A HEIGHT GREATER THAN 15 FEET SHALL BE COORDINATED WITH THE AIRPORT. LOCATION AND HEIGHTS OF STOCKPILED MATERIAL TO BE USED IN THE CONSTRUCTION SHALL BE APPROVED IN ADVANCE BY THE ENGINEER. AN FAA 7460-1 FORM WILL BE REQUIRED TO BE FILED WITH THE FAA, AND SHALL BE FILED A MINIMUM OF 45 DAYS PRIOR TO USE OF THE PROPOSED EQUIPMENT.
21. ALL CONSTRUCTION VEHICLES AND EQUIPMENT OPERATING IN THE AIRPORT OPERATIONS AREA DURING DAYLIGHT HOURS MUST BE PROVIDED WITH A FLAG ATTACHED TO THE VEHICLE SO THAT THE FLAG WILL BE READILY VISIBLE OR A 360° ROTATING AMBER BEACON. THE FLAG SHALL BE THREE FEET SQUARE CONSISTING OF AVIATION ORANGE AND WHITE SQUARED ON EACH SIDE. ALL CONSTRUCTION VEHICLES AND EQUIPMENT OPERATING ON AIRPORT PROPERTY AT NIGHT MUST BE PROVIDED WITH A 360° ROTATING/FLASHING AMBER BEACON ATTACHED TO THE VEHICLE SO THAT THE LIGHTS WILL BE READILY VISIBLE. ALL CONSTRUCTION VEHICLES AND EQUIPMENT PARKED WITHIN THE AOA AFTER A SHIFT IS COMPLETED ARE REQUIRED TO BE EQUIPPED WITH A FLAG. ALL HAUL TRUCKS ARE REQUIRED TO BE EQUIPPED WITH A FLAG AT ALL TIMES WITHIN THE AOA.
22. THE CONTRACTOR SHALL DESIGNATE A PERSON AND TWO BACKUP PEOPLE WHO CAN BE CONTACTED 24 HOURS A DAY IN THE EVENT OF AN EMERGENCY. THESE PEOPLE SHALL BE AUTHORIZED TO MAKE FIELD DECISIONS ON THE COMPANY'S BEHALF AND BE ABLE TO RESPOND WITHIN TWO HOURS OF INITIAL CONTACT.
23. ACCESS FOR AIRPORT RESCUE AND FIRE FIGHTING (ARFF) OR LOCAL RESCUE AND FIRE FIGHTING EQUIPMENT AND PERSONNEL SHALL BE MAINTAINED AT ALL TIMES. THE ARFF OR LOCAL OPERATIONS SHALL HAVE RIGHT-OF-WAY OVER ALL CONTRACTOR'S OPERATIONS AT ALL TIMES.
24. ALL WORK SHALL BE PERFORMED PER THE PROJECT PLANS AND SPECIFICATIONS AND SHALL BE COORDINATED WITH SCOTTSDALE AIRPORT OPERATIONS STAFF.

QUANTITY TABLE:

REHABILITATE GATE 1 TAXILANE AND BYPASS TAXILANE AND IMPLEMENT DRAINAGE IMPROVEMENTS					
ITEM NO.	SPEC ITEM	ITEM DESCRIPTION	UNIT	PLAN QUANTITY	AS-BUILT QUANTITY
1	C-100-14.1	CONTRACTOR QUALITY CONTROL PROGRAM	LS	1	
2	C-102-5.1	PREPARE AND IMPLEMENT STORMWATER POLLUTION PREVENTION PLAN	LS	1	
3	C-105-6.1	MOBILIZATION	LS	1	
4	SP-100-3.1	AIRFIELD SAFETY AND TRAFFIC CONTROL	LS	1	
5	SP-100-3.2	CONSTRUCTION STAKING AND SURVEY LAYOUT	LS	1	
6	SP-100-3.3	REMOVE AND REPLACE AIRCRAFT GATE LOOP DETECTOR	LS	1	
7	SP-102-5.1	CRUSHED AGGREGATE SLOPE PROTECTION ROCK, PLACE AND COMPACT	SY	120	
8	P-101-5.1	PAVEMENT REMOVAL, 2.5-INCH DEPTH	SY	1,760	
9	P-101-5.2	PAVEMENT REMOVAL, 3-INCH DEPTH	SY	1,440	
10	P-101-5.3	PAVEMENT REMOVAL, FULL DEPTH	SY	120	
11	P-401-8.1	ASPHALT SURFACE COURSE	TON	580	
12	P-603-5.1	EMULSIFIED ASPHALT TACK COAT	TON	2	
13	P-620-5.1	PAVEMENT MARKING, YELLOW WITH REFLECTIVE MEDIA, TYPE I	SF	2,220	
14	P-620-5.2	PAVEMENT MARKING, BLACK	SF	810	

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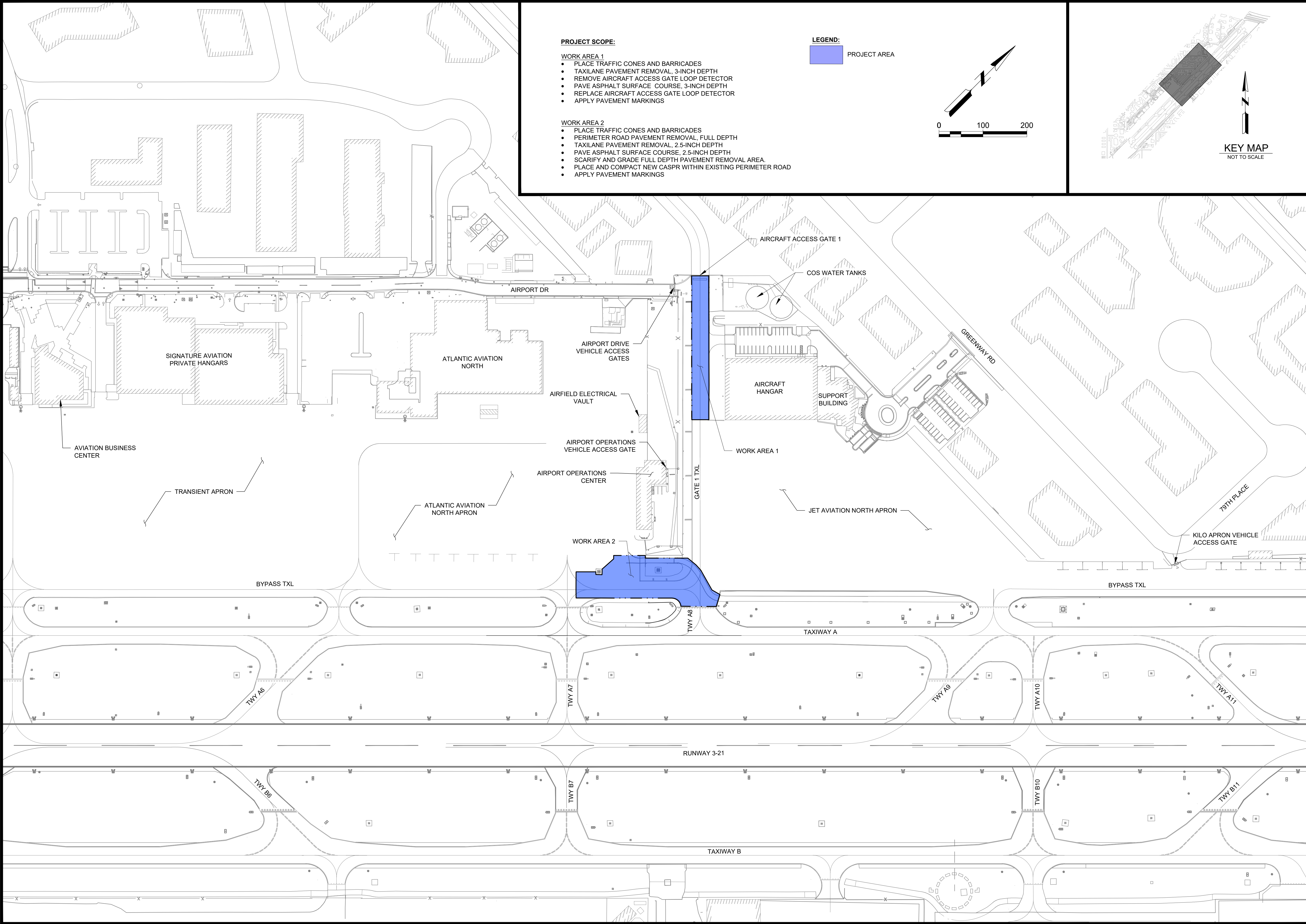
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GENERAL NOTES	

SHEET NO. 3 of 14

G-003

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PROJECT SCOPE:

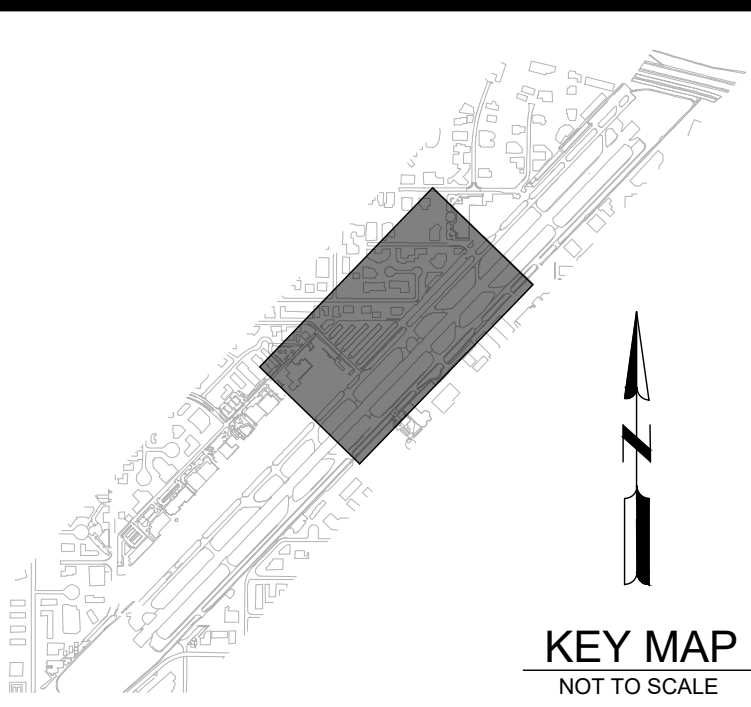
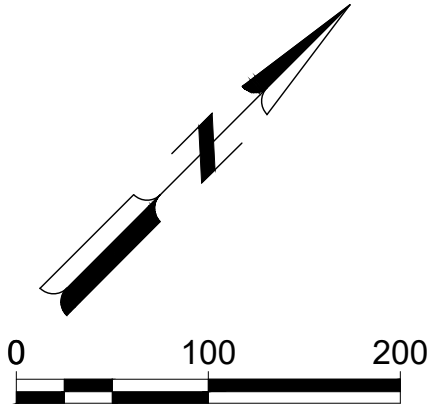
WORK AREA 1

- PLACE TRAFFIC CONES AND BARRICADES
- TAXILANE PAVEMENT REMOVAL, 3-INCH DEPTH
- REMOVE AIRCRAFT ACCESS GATE LOOP DETECTOR
- PAVE ASPHALT SURFACE, COURSE, 3-INCH DEPTH
- REPLACE AIRCRAFT ACCESS GATE LOOP DETECTOR
- APPLY PAVEMENT MARKINGS

WORK AREA 2

- PLACE TRAFFIC CONES AND BARRICADES
- PERIMETER ROAD PAVEMENT REMOVAL, FULL DEPTH
- TAXILANE PAVEMENT REMOVAL, 2.5-INCH DEPTH
- PAVE ASPHALT SURFACE COURSE, 2.5-INCH DEPTH
- SCARIFY AND GRADE FULL DEPTH PAVEMENT REMOVAL AREA.
- PLACE AND COMPACT NEW CASPR WITHIN EXISTING PERIMETER ROAD
- APPLY PAVEMENT MARKINGS

LEGEND:



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MIKULECKY

DESIGNED

ARIZONA

REGISTERED PROFESSIONAL ENGINEER

Bradley Mikulecky

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SCOTTSDALE AIRPORT

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IMPLEMENT DRAINAGE IMPROVEMENTS

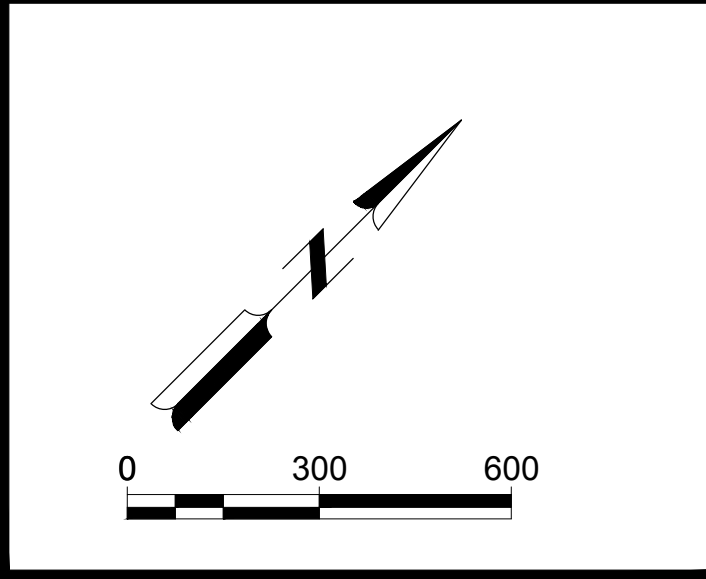
15000 N AIRPORT DRIVE

SCOTTSDALE, ARIZONA, 85260

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PROJECT CONTENTS	
PROJECT LAYOUT PLAN	
SHEET NO. 4 of 14	

G-021



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SHEET NO. 5 of 14

G-041

SURVEY FOR THE KILO APRON WAS PERFORMED BY TRACE CONSULTING, LLC IN DECEMBER 2024.

NGS CONTROL POINT DESIGNATED SDL D, PID AA3551 (PAC), A BRASS CAP LOCATED AT THE SCOTTSDALE AIRPORT, NORTH OF THE CITY OF SCOTTSDALE, AZ WHICH IS 14 MILES NORTHEAST OF THE INTERSECTION OF I-10 AND I-17. STATION IS LOCATED IN THE MIDDLE SECTION OF THE AIRPORT BETWEEN THE RUNWAY AND PARALLEL TAXIWAY A HAVING AN ELEVATION OF 1470.01, NAVD88 VERTICAL DATUM. CONTRACTOR TO VERIFY LOCAL SURVEY CONTROL POINTS AND PROJECT BENCHMARK PRIOR TO START OF CONSTRUCTION.

PRIMARY PROJECT BENCHMARK USED:
NGS BC SDL D (PAC)
NORTHING: 954234.205
EASTING: 701710.22
ELEVATION: 1470.01

PRIMARY HORIZONTAL CONTROL:
NORTH END THRESHOLD MONUMENT
NORTHING: 957123.510
EASTING: 704729.520

SOUTH END THRESHOLD MONUMENT
NORTHING: 951188.460
EASTING: 699000.650

CONTROL POINTS				
CONTROL POINT	NORTHING	EASTING	ELEVATION	DESCRIPTION
6	955797.270	702889.810	1490.59	PK NAIL
101	957123.510	704729.520	1510.52	NORTH END THRESHOLD MONUMENT
102	951188.460	699000.650	1444.42	SOUTH END THRESHOLD MONUMENT
103	954234.205	701710.220	1470.01	NGS BC SDL D (PAC) (BRASS CAP)

ALIGNMENT TABLE								
LINE #	LENGTH	BEARING	START STATION	END STATION	ALIGNMENT START NORTHING	ALIGNMENT END EASTING	ALIGNMENT END NORTHING	ALIGNMENT END EASTING
L1	500.00	N43° 59' 02.21"E	10+00.00	15+00.00	N: 954020.19	E: 701240.88	N: 954379.96	E: 701588.10
L2	500.00	S46° 00' 57.79"E	0+00.00	5+00.00	N: 954894.46	E: 700952.62	N: 954547.23	E: 701312.38

GENERAL PHASING NOTES:

1. THE MAXIMUM HEIGHT OF CONSTRUCTION EQUIPMENT IS 15-FEET. CONTRACTOR SHALL COORDINATE WITH ENGINEER AND AIRPORT OPERATIONS DURING THE MOBILIZATION PHASE OF THE PROJECT IF EQUIPMENT WILL EXCEED THE MAXIMUM HEIGHT REQUIREMENT.
2. ALL CONSTRUCTION VEHICLES AND EQUIPMENT OPERATING ON AIRPORT PROPERTY OUTSIDE DAYTIME HOURS SHALL BE PROVIDED WITH A 360 DEGREE FLASHING AMBER BEACON ATTACHED TO THE VEHICLE SO THAT THE LIGHTS WILL BE READILY VISIBLE.
3. ALL CONSTRUCTION VEHICLES AND EQUIPMENT PARKED WITHIN THE AOA AFTER A SHIFT IS COMPLETED SHALL BE EQUIPPED WITH A FLAG AND PARKED OUTSIDE THE ROFA.
4. ALL HAUL TRUCKS SHALL BE EQUIPPED WITH A FLAG AND A 360 DEGREE FLASHING AMBER BEACON AT ALL TIMES WITHIN THE AOA.
5. ALL HAUL ROUTES NOT ON ACTIVE PAVEMENT SHALL BE DELINEATED WITH TRAFFIC CONES, UNLESS OTHERWISE APPROVED BY AIRPORT OPERATIONS STAFF.
6. ALL PAVEMENT USED FOR HAULING SHALL BE SWEEP/VACUUMED PRIOR TO OPENING TO AIRCRAFT TRAFFIC AFTER EACH SHIFT OR TRUCK HAUL, AS APPROVED BY AIRPORT OPERATIONS.
7. EXACT LOCATIONS OF THE PLACEMENT OF BARRICADES SHALL BE AS APPROVED BY THE AIRPORT.
8. PARKING FOR CONSTRUCTION PERSONNEL SHALL BE IN THE PARKING LOT OF THE AVIATION BUSINESS CENTER.
9. CONTRACTOR SHALL BUILD TEMPORARY SLOPES AS NECESSARY TO MEET GRADING STANDARDS INSIDE THE PROJECT AREA. ALL COSTS TO BE INCLUDED IN THE AIRFIELD SAFETY AND TRAFFIC CONTROL BID ITEM.
10. A VACUUM SWEEPER MUST BE MAINTAINED ONSITE FOR THE PROJECT DURATION AND BE CAPABLE OF PICKING UP DEBRIS ON PAVED SURFACES. ELGIN CROSSWIND OR EQUIVALENT. THE VACUUM SWEEPER IS REQUIRED TO HAVE NYLON BRISTLES. A VACUUM SWEEPER WITH METAL BRISTLES IS NOT ALLOWED.
11. CONTRACTOR STAGING AREA SHALL BE WITHIN PHASE LIMITS AND SHALL UTILIZE THE AIRPORT DRIVE VEHICLE ACCESS GATE FOR AIRFIELD ACCESS. CONTRACTOR SHALL COORDINATE WITH AIRPORT OPERATIONS TO COMPLETE REQUIRED AIRFIELD DRIVING SAFETY TRAINING FOR ALL CONTRACTOR PERSONNEL EXPECTED TO ESCORT HAULING TRUCKS ON AIRFIELD. CONTRACTOR IS RESPONSIBLE FOR ANY EQUIPMENT, GATE, PAVEMENT, OR FENCE DAMAGES CAUSED BY HAULING.
12. CONTRACTOR SHALL COORDINATE CONSTRUCTION SCHEDULE WITH AIRPORT OPERATIONS AND ENGINEER BEFORE SECOND APPLICATION OF PAVEMENT MARKINGS. THIS IS CONSIDERED INCIDENTAL TO PAVEMENT MARKING APPLICATION.
13. TOTAL CONTRACT TIME IS 6 CONSECUTIVE CALENDAR DAYS.

PHASE NOTES:

1. CONTRACTOR STAGING AREA SHALL BE WITHIN WORK AREA 1 OUTSIDE OF THE ROFA.
2. CONTRACTOR SHALL COMPLETE CONSTRUCTION OF WORK AREA 1 AND WORK AREA 2 CONCURRENTLY.
3. CONTRACTOR EXPECTED WORK HOURS ARE DURING DAYTIME BETWEEN HOURS OF FROM 0600 - 2000.

WORK AREA 1:

- PLACE TRAFFIC CONES AND BARRICADES
- TAXILANE PAVEMENT REMOVAL, 3-INCH DEPTH
- REMOVE AND REPLACE AIRCRAFT ACCESS GATE LOOP DETECTOR
- PAVE ASPHALT SURFACE COURSE, 3-INCH DEPTH
- APPLY PAVEMENT MARKINGS

WORK AREA 2:

- PLACE TRAFFIC CONES AND BARRICADES
- PERIMETER ROAD PAVEMENT REMOVAL, FULL DEPTH
- TAXILANE PAVEMENT REMOVAL, 2.5-INCH DEPTH
- PAVE ASPHALT SURFACE COURSE, 2.5-INCH DEPTH
- SCARIFY AND GRADE FULL DEPTH PAVEMENT REMOVAL AREA
- PLACE AND COMPACT NEW CASPR WITHIN EXISTING PERIMETER ROAD
- APPLY PAVEMENT MARKINGS

TOTAL DURATION:

- 6 CONSECUTIVE CALENDAR DAYS

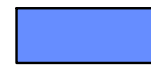
RUNWAY CLOSURES:

- NONE

OPERATIONAL IMPACT:

- VEHICLE TRAFFIC THROUGH PROJECT LIMITS PROHIBITED
- CONTRACTOR SHALL OBTAIN ESCORT WHEN TRAVELING BETWEEN WORK AREA 1 AND WORK AREA 2
- CONTRACTOR SHALL YIELD TO AIRCRAFT OPERATIONS WHEN TRAVELING BETWEEN WORK AREA 1 AND WORK AREA 2

LEGEND:



PHASE 1



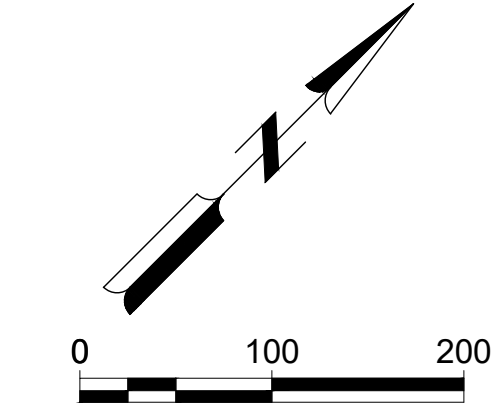
CONTRACTOR HAUL ROUTE



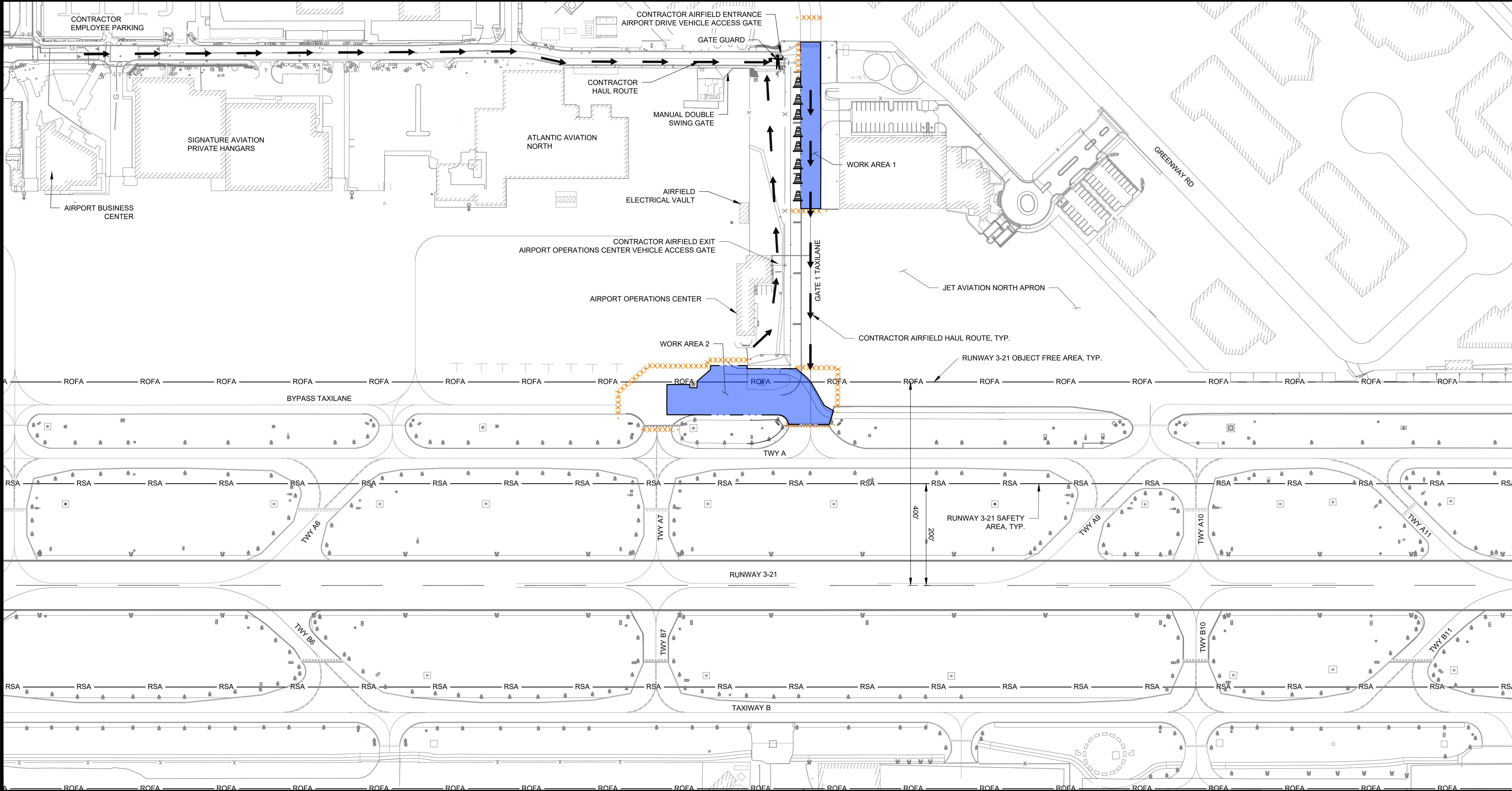
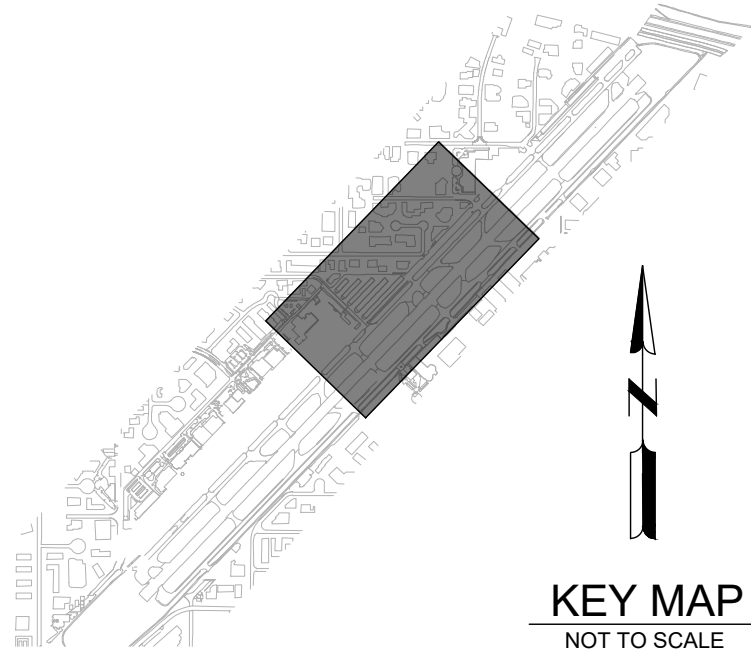
CONTRACTOR PROVIDED
LOW PROFILE BARRICADES,
SEE DETAIL 1/G-082



AIRPORT PROVIDED TRAFFIC CONE

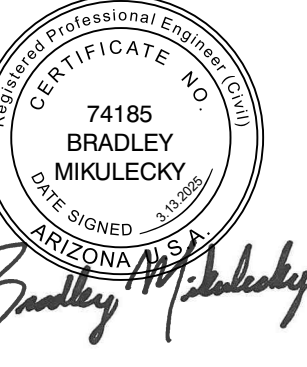


KEY MAP
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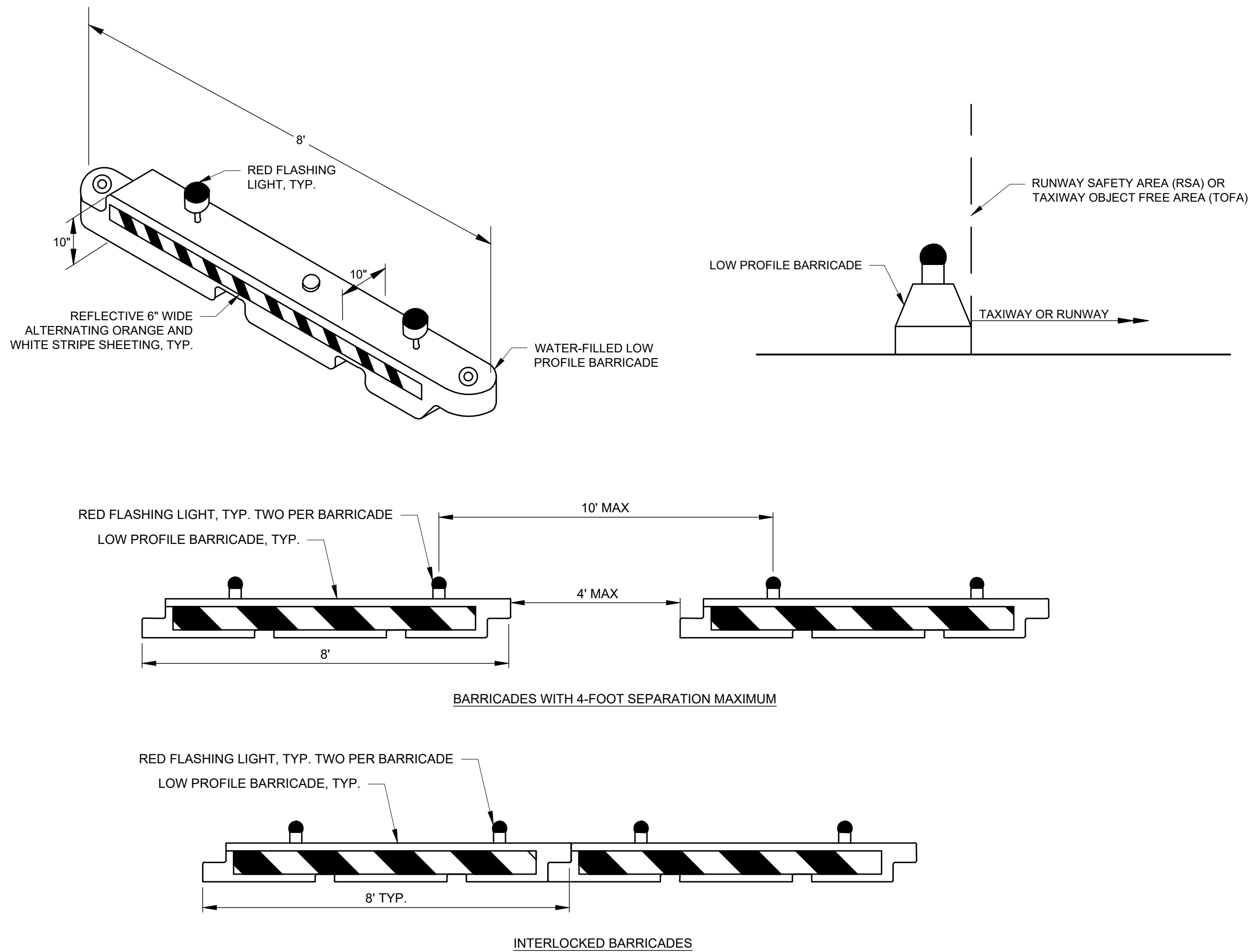
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SHEET CONTENTS
CONSTRUCTION
SAFETY AND PHASING
PLAN

SHEET NO. 6 of 14

G-081

X:\2886300\242372.01\TECH\CDRAWINGS\G-082 CONSTRUCTION SAFETY AND PHASING DETAILS.DWG
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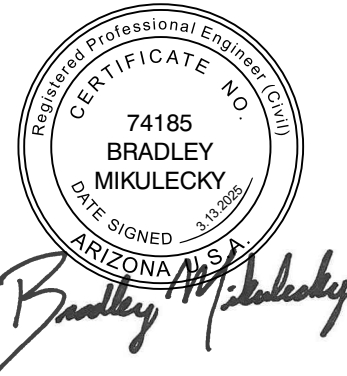


- NOTES:
1. CONTRACTOR SHALL DETERMINE THE NUMBER OF LOW PROFILE BARRICADES AND LIGHTS REQUIRED TO COMPLETE THE PROJECT BASED ON THE PHASING REQUIREMENTS SHOWN ON THESE PLANS. CONTRACTOR SHALL CONFIRM BARRICADE LOCATIONS AT THE PRE-CONSTRUCTION MEETING.
 2. INSTALL LOW PROFILE BARRICADES WHERE INDICATED ON PLANS OR AS INSTRUCTED BY THE RPR OR AIRPORT OPERATIONS.
 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING, MOVING, LIGHTING, AND MAINTAINING THE BARRICADES DURING THE ENTIRE PROJECT.
 4. BARRICADES SHALL BE FILLED WITH WATER AT ALL TIMES DURING CONSTRUCTION AND WEIGHTED AGAINST JET BLAST.
 5. INSTALL SOLAR/BATTERY POWERED LIGHTS EQUALLY SPACED PER EACH BARRICADE AS SHOWN ON THESE PLANS. LIGHTS SHALL MAINTAIN SUCH INTENSITY SO AS TO BE READILY IDENTIFIED FROM DISTANCES OF 200' OR GREATER DURING PERIODS OF DARKNESS.
 6. BARRICADES SHALL BE PLACED ACROSS ENTIRE BYPASS TAXILANE PAVEMENT TO PREVENT AIRCRAFT AND VEHICLE ENTRANCE TO PROJECT AREA.
 7. BARRICADES SHALL BE MAINTAINED IN GOOD CONDITION AND FREE OF LEAKS AT ALL TIMES. CONTRACTOR TO REPLACE LEAKING OR DAMAGED BARRICADES AT THEIR EXPENSE.
 8. CONTRACTOR SHALL ASSUME ALL BARRICADES ARE INTERLOCKED, HOWEVER, CONTRACTOR TO CONFIRM WITH THE AIRPORT ON THE LOCATION OF INTERLOCKED BARRICADES VERSUS BARRICADES WITH SPACING.

1 **LOW PROFILE BARRICADE**
NO SCALE

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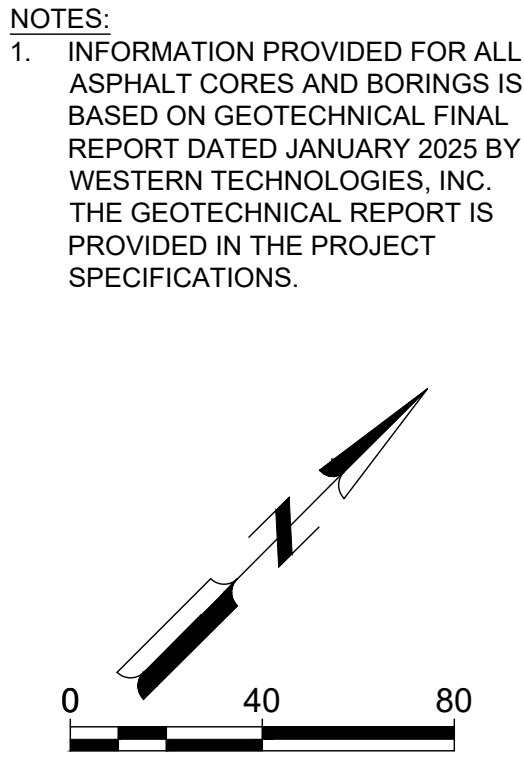
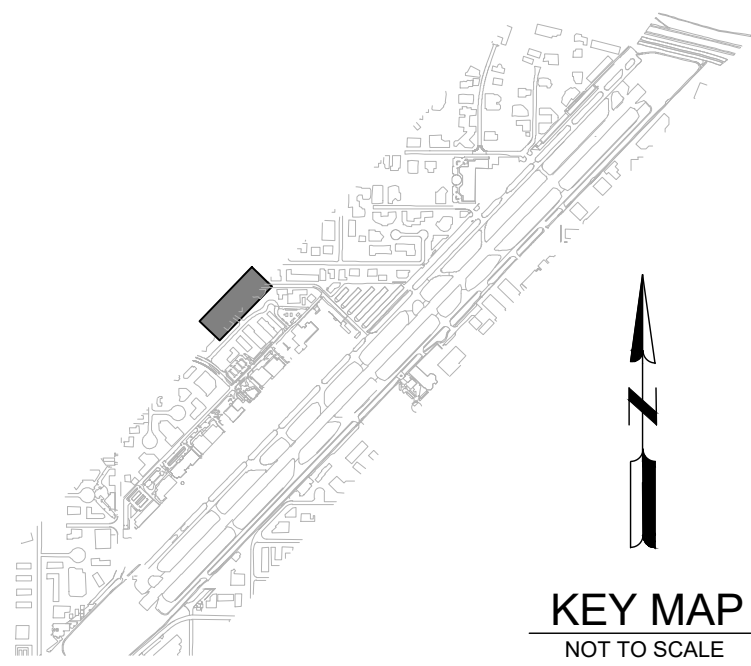
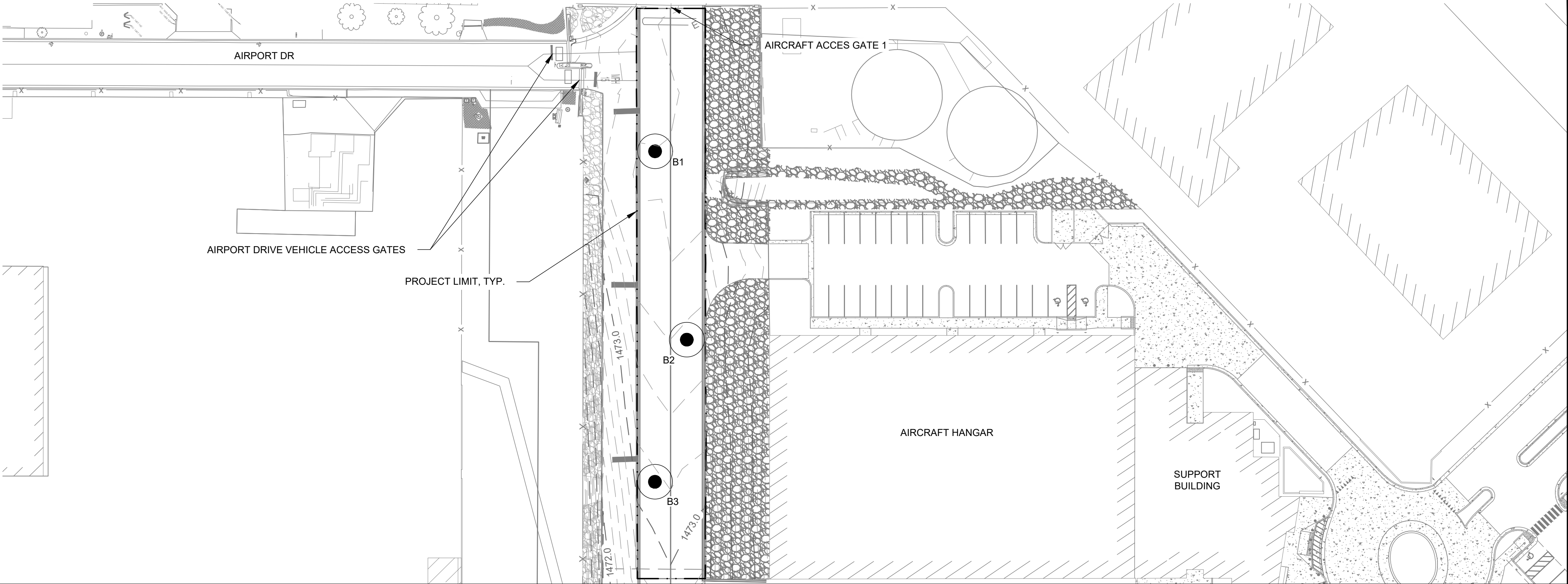
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SHEET CONTENTS
**CONSTRUCTION
SAFETY AND PHASING
DETAILS**

SHEET NO. 7 of 14

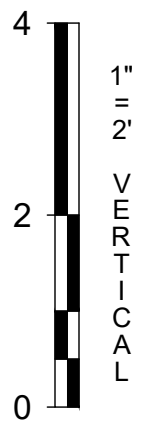
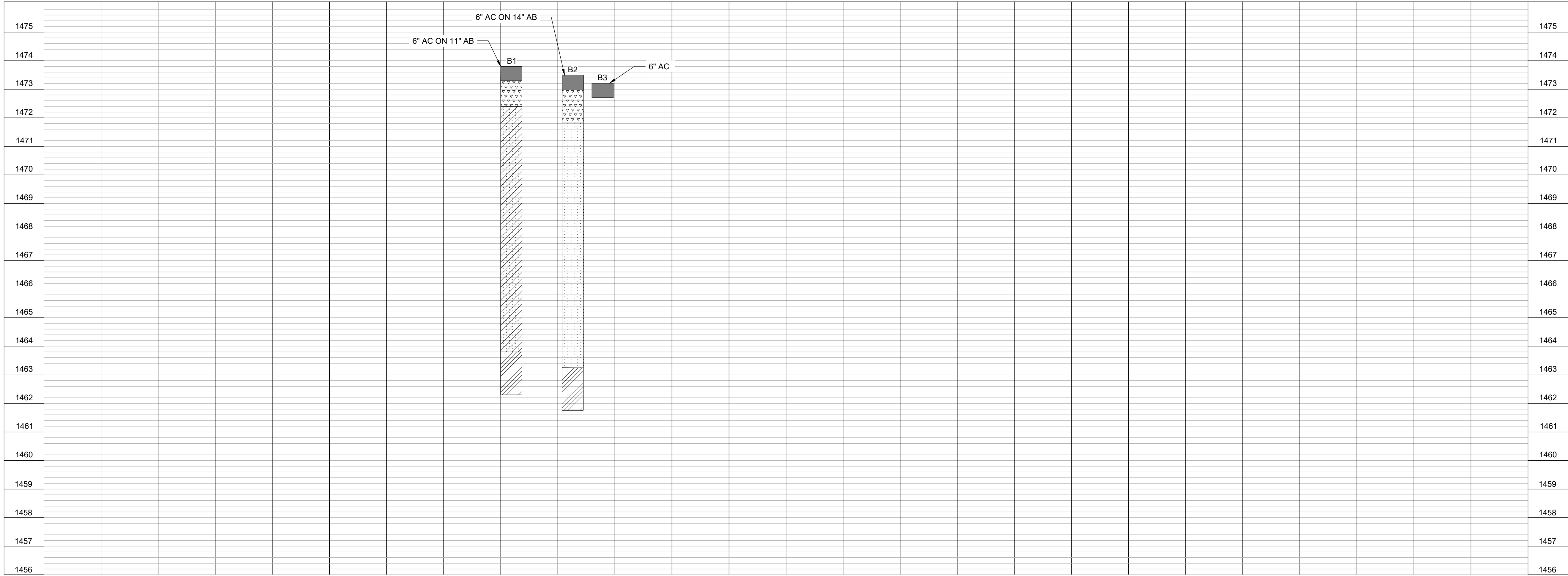
G-082



NOTES:
1. INFORMATION PROVIDED FOR ALL ASPHALT CORES AND BORINGS IS BASED ON GEOTECHNICAL FINAL REPORT DATED JANUARY 2025 BY WESTERN TECHNOLOGIES, INC. THE GEOTECHNICAL REPORT IS PROVIDED IN THE PROJECT SPECIFICATIONS.

SOILS BORING LEGEND

- BORING / CORING LOCATION
- ASPHALT
- AGGREGATE BASE COURSE
- SILTY CLAYEY SAND
- CLAYEY SAND
- CLAYEY SAND WITH GRAVEL
- SANDY LEAN CLAY



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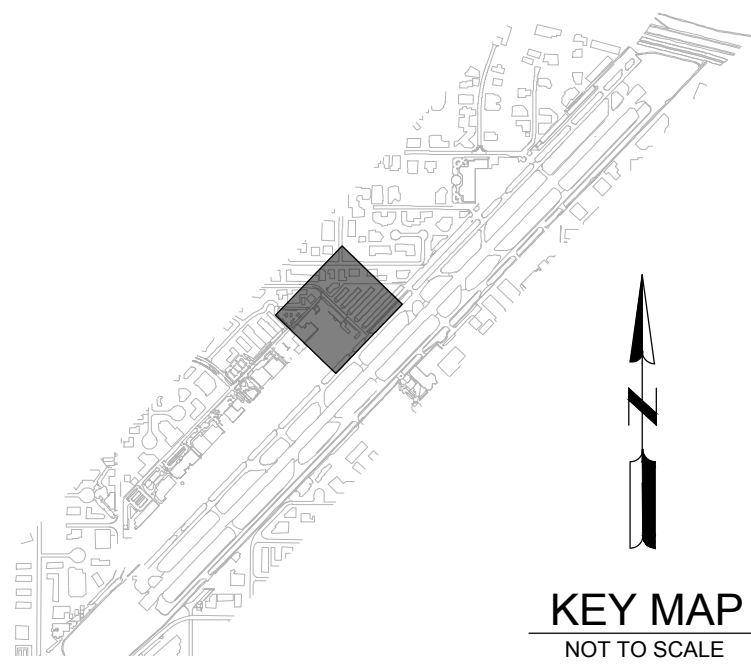
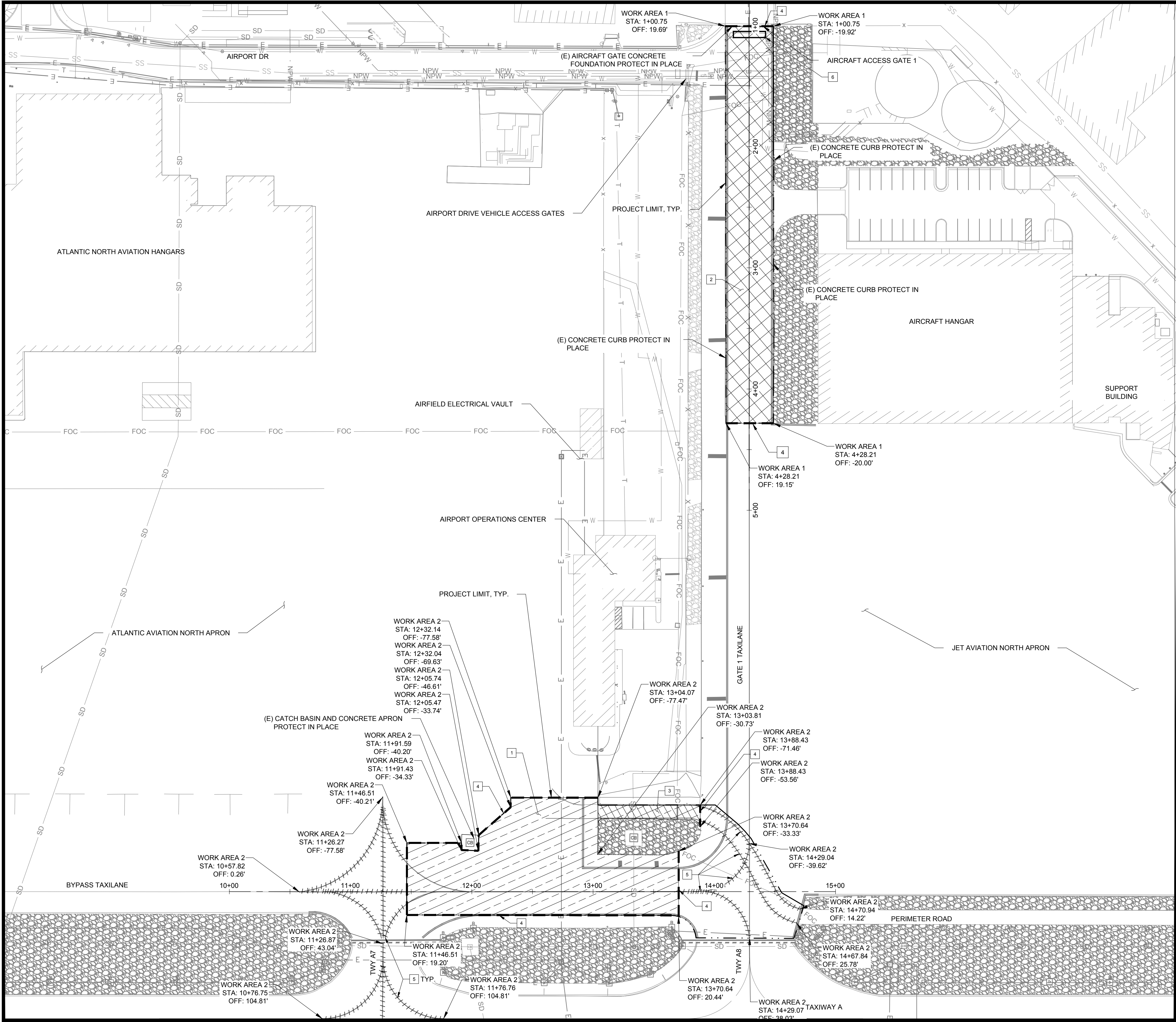
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SHEET CONTENTS
PLAN AND LOG OF
SOIL BORINGS

SHEET NO. 8 of 14

B-051



- DEMOLITION KEYNOTES:
- 1 PAVEMENT REMOVAL, 2.5-INCH DEPTH, SEE DETAIL 3/C-301
 - 2 PAVEMENT REMOVAL, 3-INCH DEPTH, SEE DETAIL 1/C-301
 - 3 PAVEMENT REMOVAL, FULL DEPTH
 - 4 SAW CUT PAVEMENT
 - 5 BLACK OUT (E) PAVEMENT MARKING
 - 6 REMOVE (E) AIRCRAFT ACCESS GATE LOOP DETECTOR

- DEMOLITION PLAN LEGEND:
- PAVEMENT REMOVAL, 2.5-INCH DEPTH
 - PAVEMENT REMOVAL, 3-INCH DEPTH
 - PAVEMENT REMOVAL, FULL DEPTH
 - SAW CUT

- DEMOLITION PLAN NOTES:
1. BLACK OUT EXISTING PAVEMENT MARKINGS WILL BE PAID THROUGH BID ITEM P-620-5.2.
 2. CONTRACTOR SHALL CONFIRM THE STATION OFFSET LOCATIONS WITH ENGINEER IN THE FIELD PRIOR TO DEMOLITION.

BLACK OUT (E) PAVEMENT MARKING. SEE NOTE 1.

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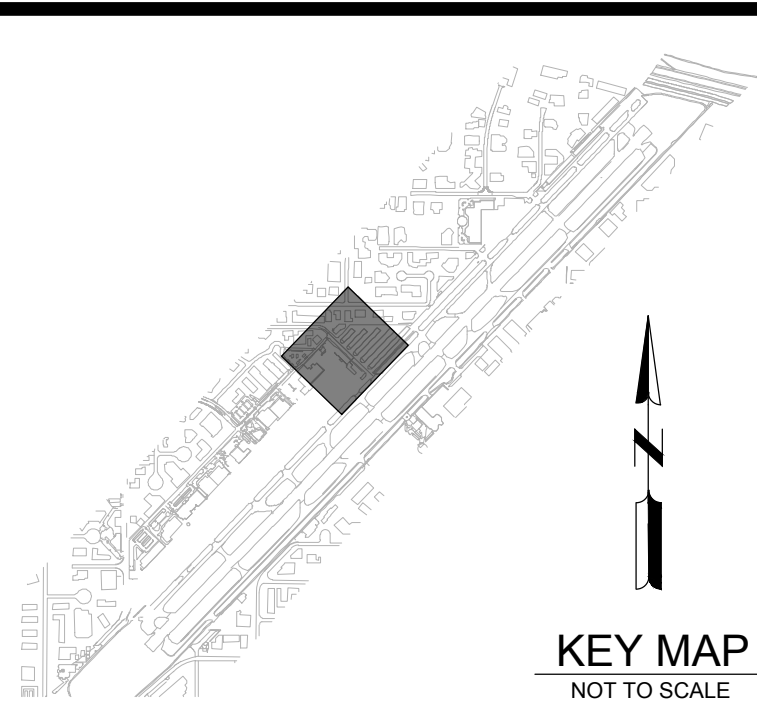
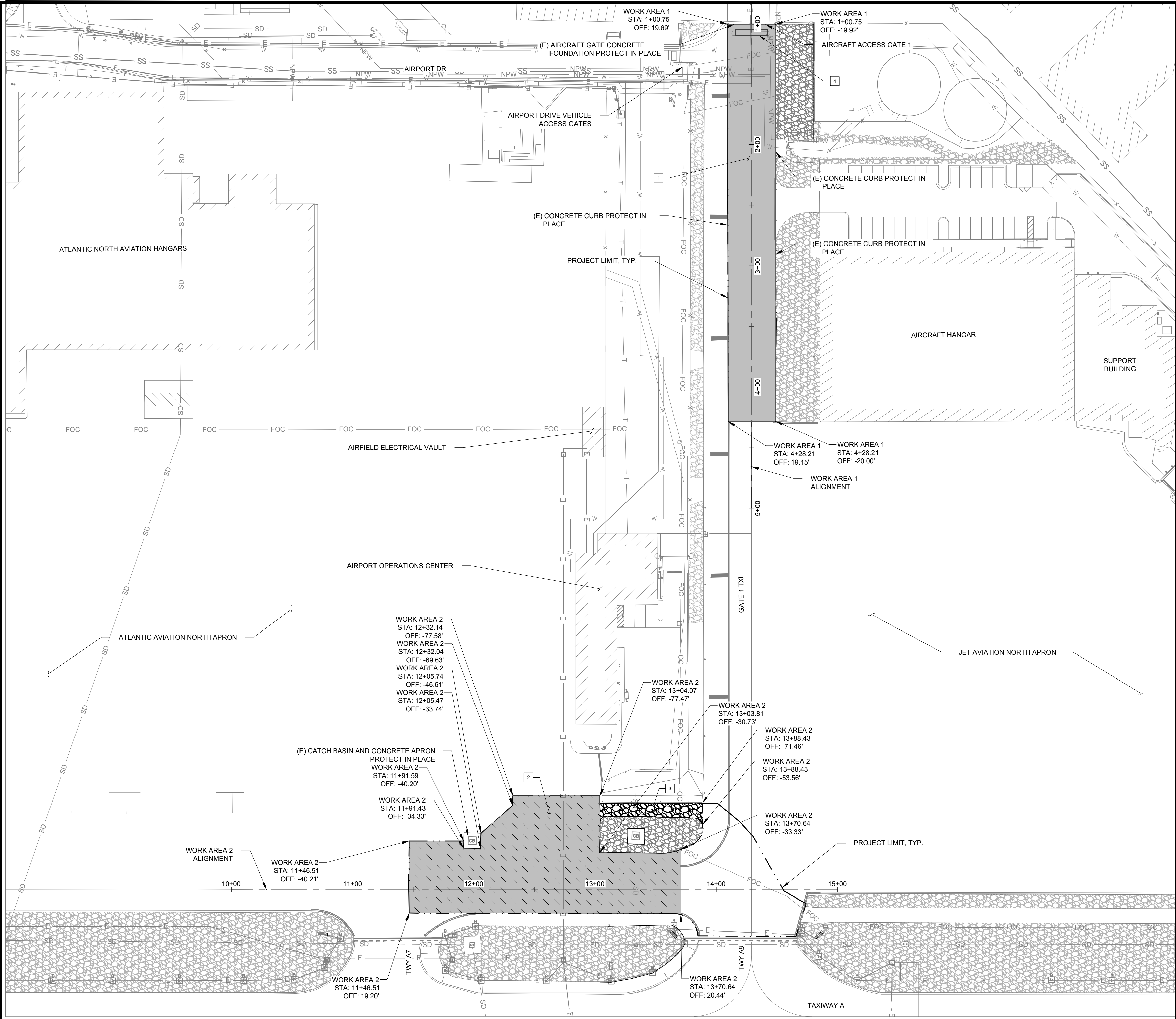
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SHEET CONTENTS
DEMOLITION PLAN

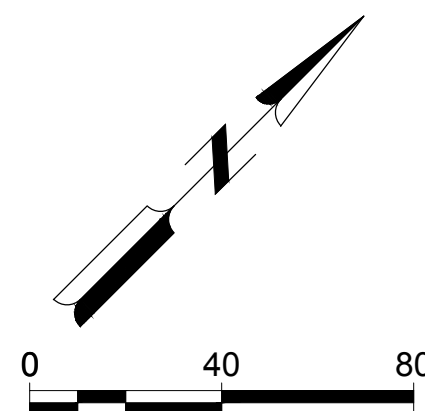
SHEET NO. 10 of 14

C-051



- GRADING AND DRAINAGE KEYNOTES:
- 3-INCH ASPHALT OVERLAY (P-401), SEE DETAIL 4/C-301
 - 2.5-INCH ASPHALT OVERLAY (P-401), SEE DETAIL 2/C-301
 - PLACE (N) CASPR, SEE DETAIL 5/C-301
 - REPLACE AIRCRAFT ACCESS GATE LOOP DETECTOR

- GRADING AND DRAINAGE LEGEND:
- 3-INCH ASPHALT OVERLAY (P-401)
 - 2.5-INCH ASPHALT OVERLAY (P-401)
 - PLACE AND COMPACT (N) CASPR
 - PROJECT LIMIT LINE



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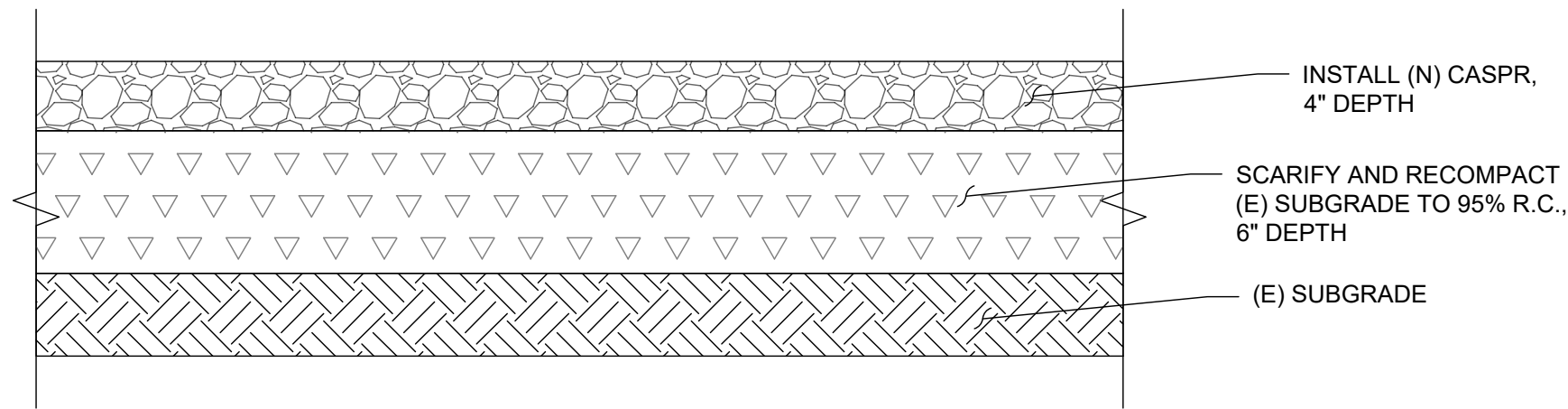
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SHEET CONTENTS
GRADING AND
DRAINAGE PLAN

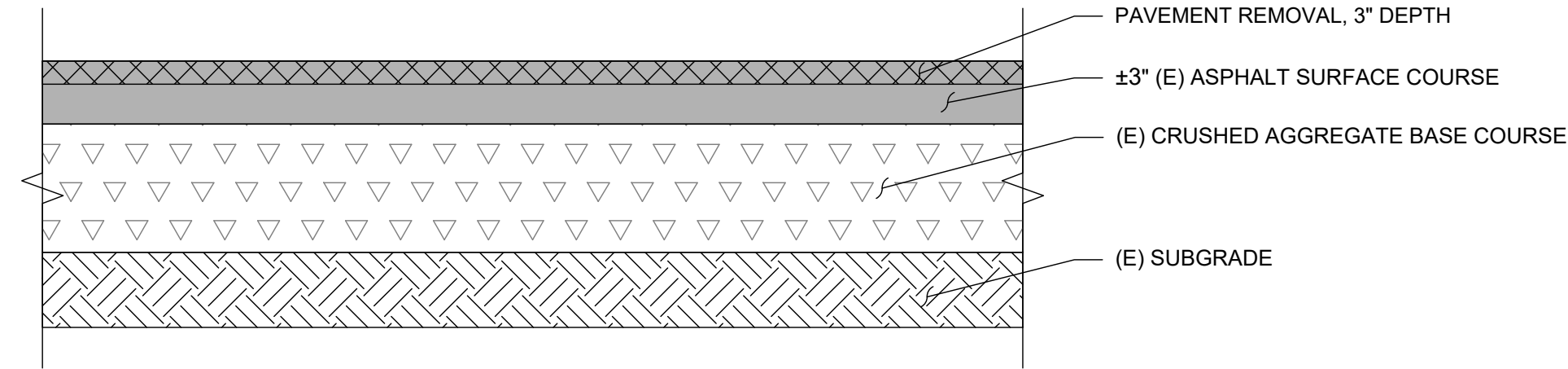
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C-101

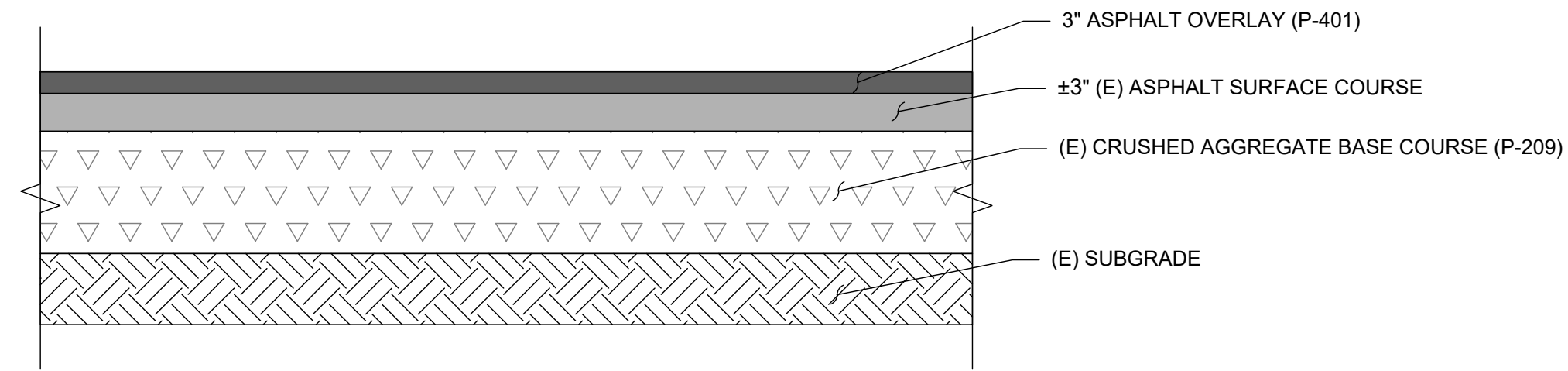
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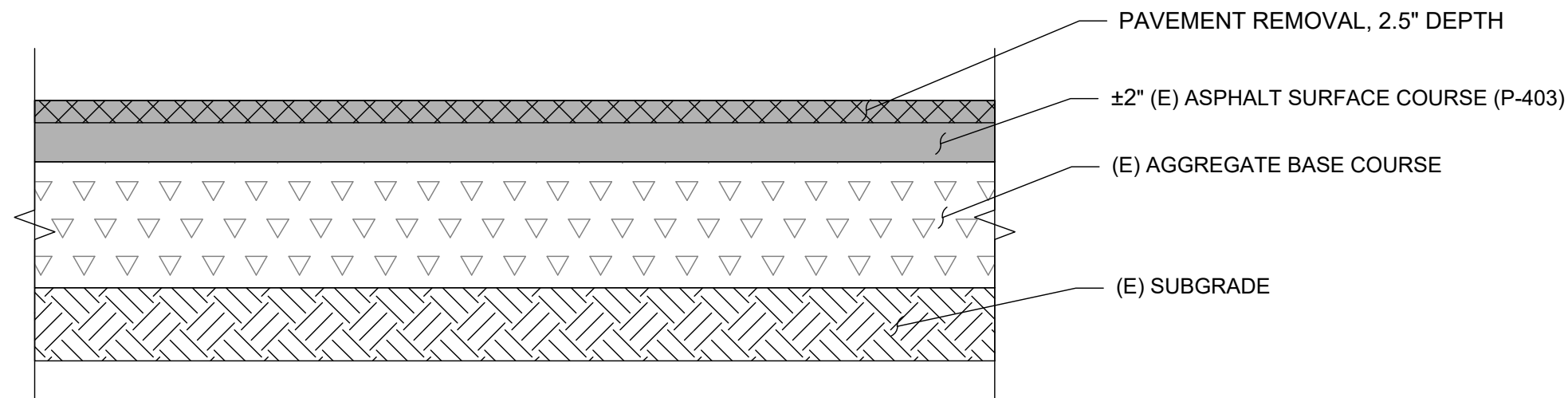
5 PROPOSED CASPR SECTION
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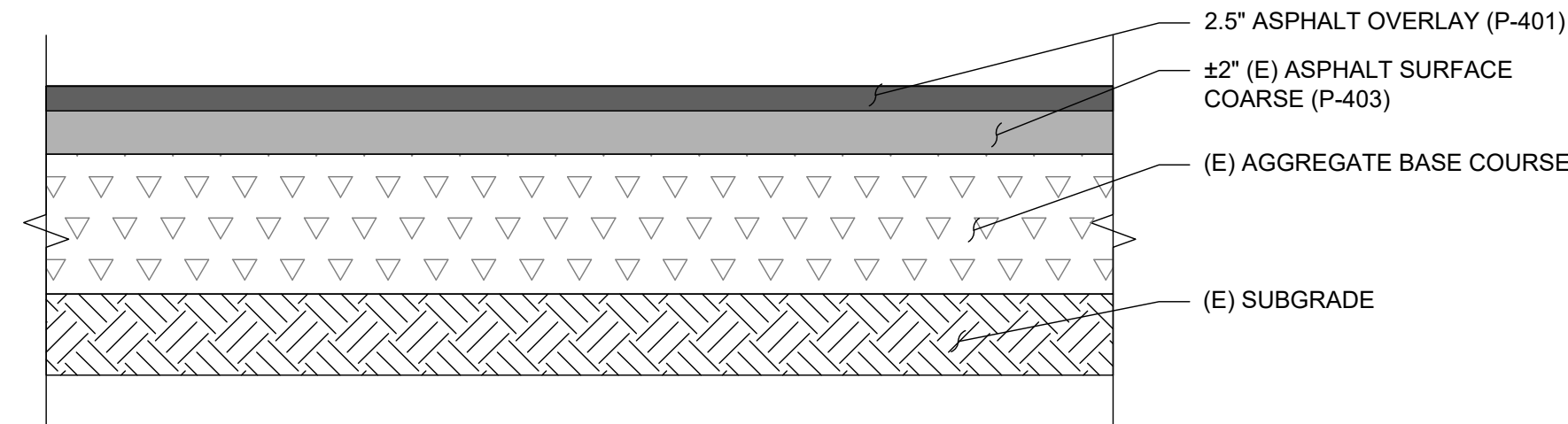
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NO SCALE



2 PROPOSED GATE 1 TAXILANE
PAVEMENT SECTION
NO SCALE



3 EXISTING BYPASS TAXILANE
NO SCALE



4 PROPOSED BYPASS TAXILANE
NO SCALE

GENERAL NOTE:
1. FOR ALL PROPOSED PAVEMENT SECTIONS BEING CONSTRUCTED,
CONTRACTOR SHALL HAVE A 1-FOOT OVERBUILD OF THE SUBBASE
AND BASE COURSE.

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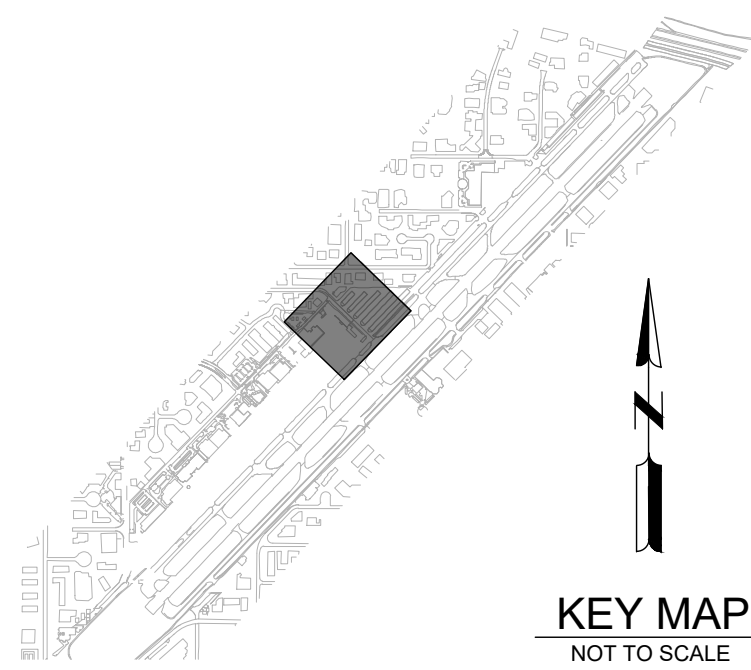
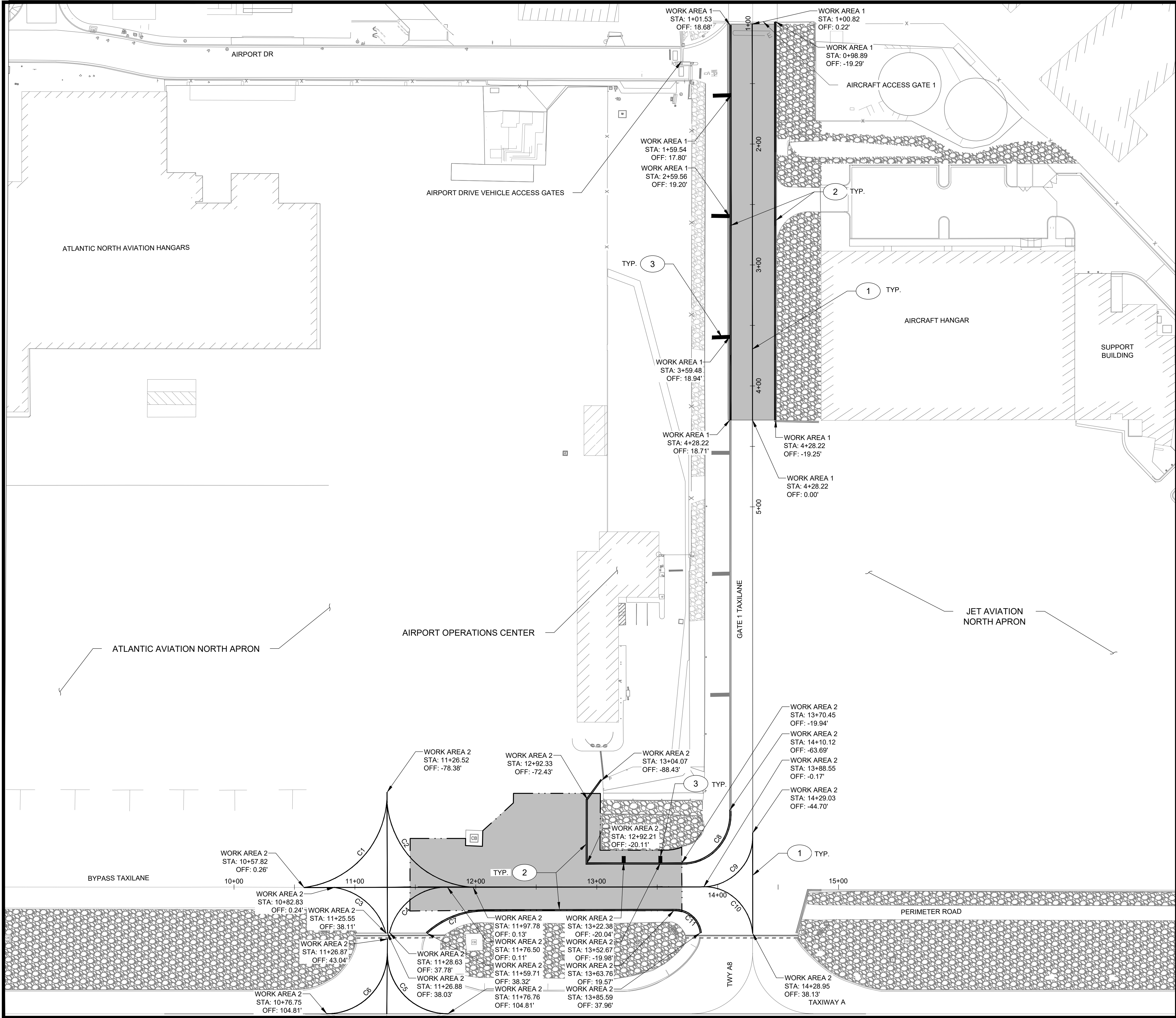
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SHEET CONTENTS
TYPICAL SECTIONS
AND CIVIL DETAILS

SHEET NO. 12 of 14

C-301

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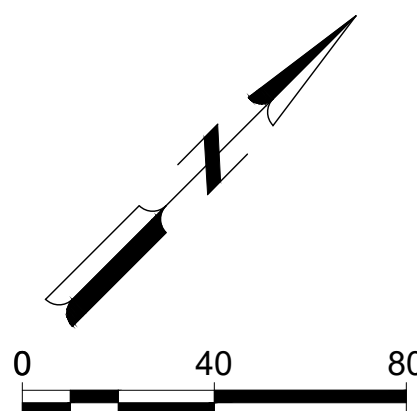


MARKING KEYNOTES

- 1 TAXILANE CENTERLINE MARKING, SEE DETAIL 1/C-671
- 2 TAXILANE EDGE MARKING, SEE DETAIL 2/C-671
- 3 TAXILANE SHOULDER MARKING, SEE DETAIL 3/C-671

CURVE DATA

SEGMENT	LENGTH	RADIUS	DELTA
C1	110	110	83.79
C2	118	118	89.96
C3	61	61	69.63
C4	68	68	77.50
C5	78	78	89.48
C6	78	78	89.47
C7	54	54	41.51
C8	67	67	95.65
C9	65	65	90.32
C10	60	60	83.70
C11	34	34	87.85



Mead & Hunt

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SCOTTSDALE AIRPORT
REHABILITATE GATE 1 TAXILANE AND BYPASS TAXILANE AND
IMPLEMENT DRAINAGE IMPROVEMENTS
15000 N AIRPORT DRIVE
SCOTTSDALE, ARIZONA, 85260

ISSUED
NOT FOR CONSTRUCTION

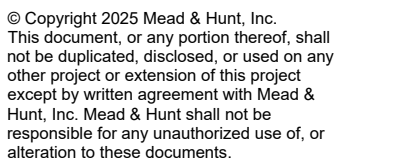
ISSUE FOR BID

AIP NO.: 3-04-0032-049-2025
MSB NO.: 2886300-242372.01
DATE: MARCH 2025
DESIGNED BY: CLH
DRAWN BY: CLH
CHECKED BY: BJM
DO NOT SCALE DRAWINGS

SHEET CONTENTS
MARKING PLAN

SHEET NO. 13 of 14

C-651



SCOTTSDALE AIRPORT
REHABILITATE GATE 1 TAXILANE AND BYPASS TAXILANE AND
IMPLEMENT DRAINAGE IMPROVEMENTS
15000 N AIRPORT DRIVE
SCOTTSDALE, ARIZONA; 85260

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AIP NO.: 3-04-0032-049-2025
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SHEET CONTENTS

MARKING DETAILS

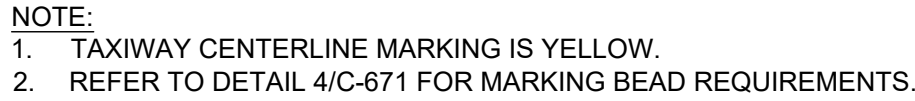
SHEET NO. 14 of 14

C-671

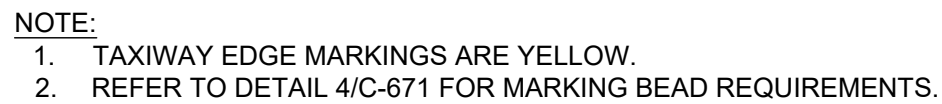
NOTE:

1. BLACK BORDERS DO NOT CONTAIN GLASS BEADS. ONLY THE PAINT WITHIN THE BORDERS MAY CONTAIN GLASS BEADS.
2. ALL YELLOW PAINT SHALL RECEIVE TWO COATS OF PAINT AND GLASS BEADS, PER P-620 SPECIFICATION.
3. ALL BLACK PAINT SHALL RECEIVE ONE COAT OF PAINT AND NO GLASS BEADS.

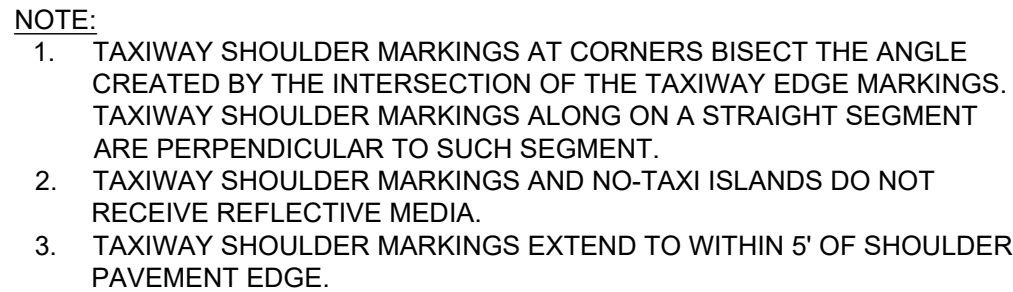
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NO SCALE



3